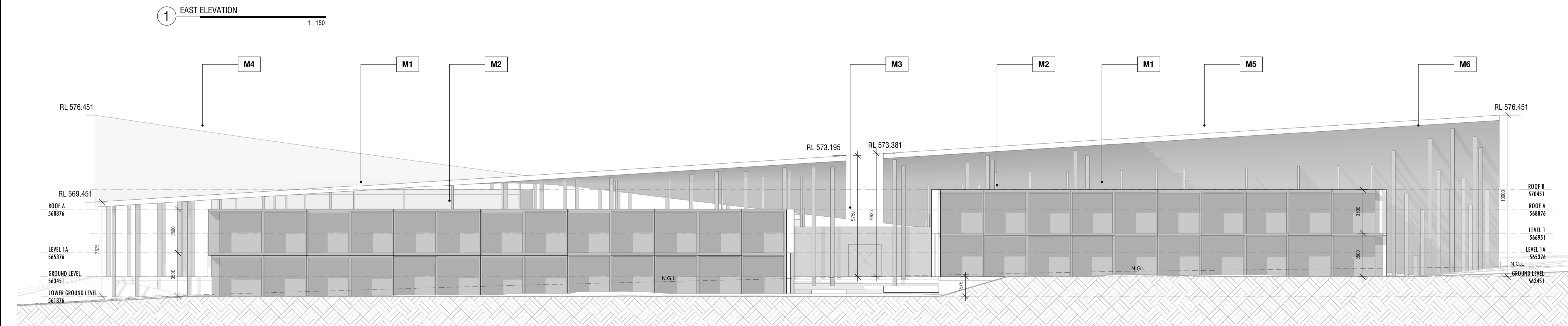
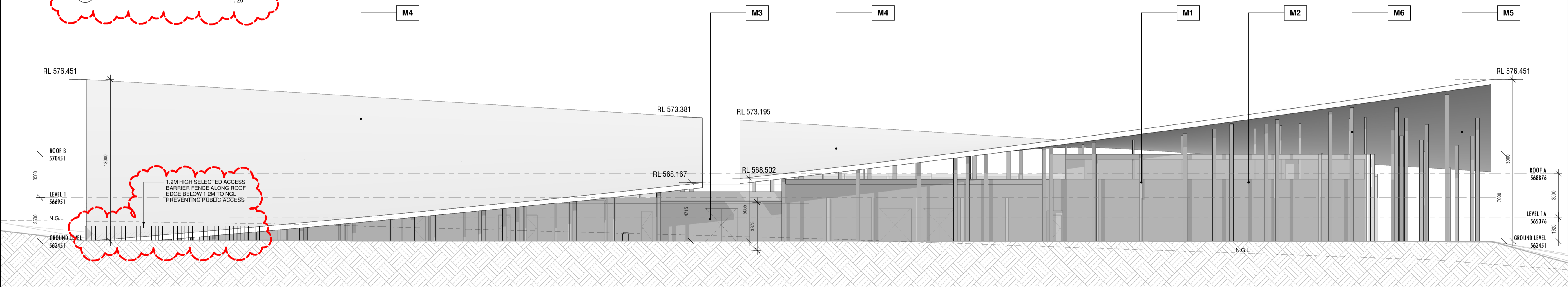


- EXTERNAL MATERIAL & FINISHES**
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 - M2** SELECTED POWDER COATED ALUMINIUM FRAME WINDOW - CLEAR GLASS
 - M3** SELECTED GLASS CURTAIN WALL
 - M4** BIOPHILIC / GREEN ROOF - SELECTED PLANTS REFER TO LANDSCAPE PLAN
 - M5** SELECTED TIMBER CLADDING SOFFIT FINISH
 - M6** SELECTED TIMBER CLADDING COLUMN FINISH

3 TYPICAL DETAIL - BIOPHILIC GREEN ROOF
1:20



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Revision
P1 - PRELIMINARY ISSUE
A - TOWN PLANNING RP1

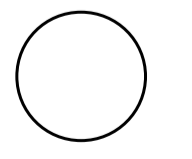
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Client
FORTE GROUP PTY LTD

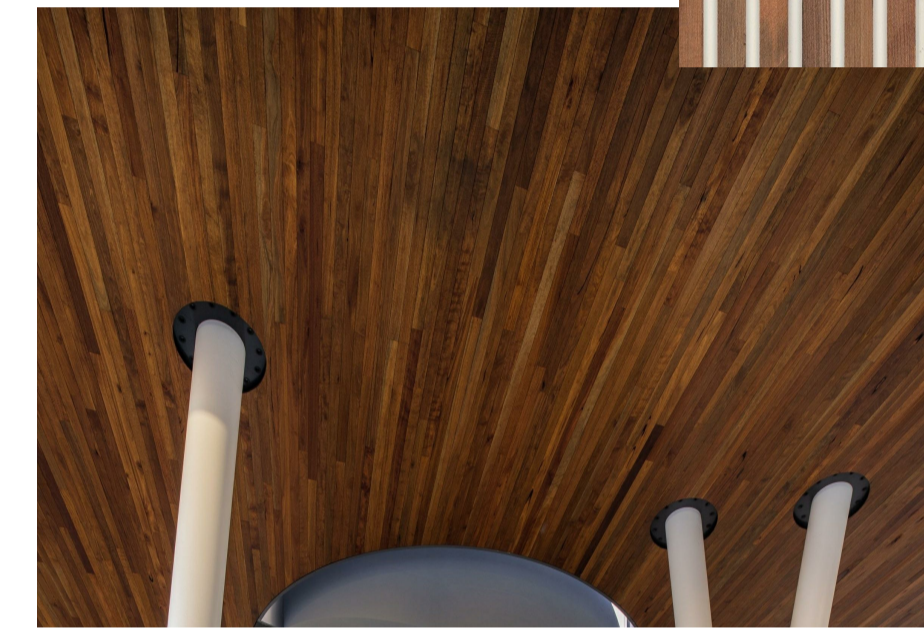
Project
**AGRICULTURAL HOTEL DEVELOPMENT
63 MAHERS ROAD, WARRENHEIP**

Drawing Title
ELEVATIONS - EAST & WEST
Scale: As indicated @ A1
Date: 21.10.2021

Issue
TOWN PLANNING APPLICATION
Drawn: CH
Drawing No: TPA04.02
Job No: 21-02
Revision: R1



- M1** SELECTED TIMBER CLADDING WALL FINISH
- M2** SELECTED POWDER COATED ALUMINIUM FRAME WINDOW - CLEAR GLASS
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- M1**
- M5**
- M6**



M2

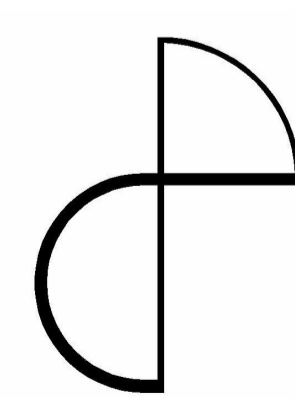


M4



M3

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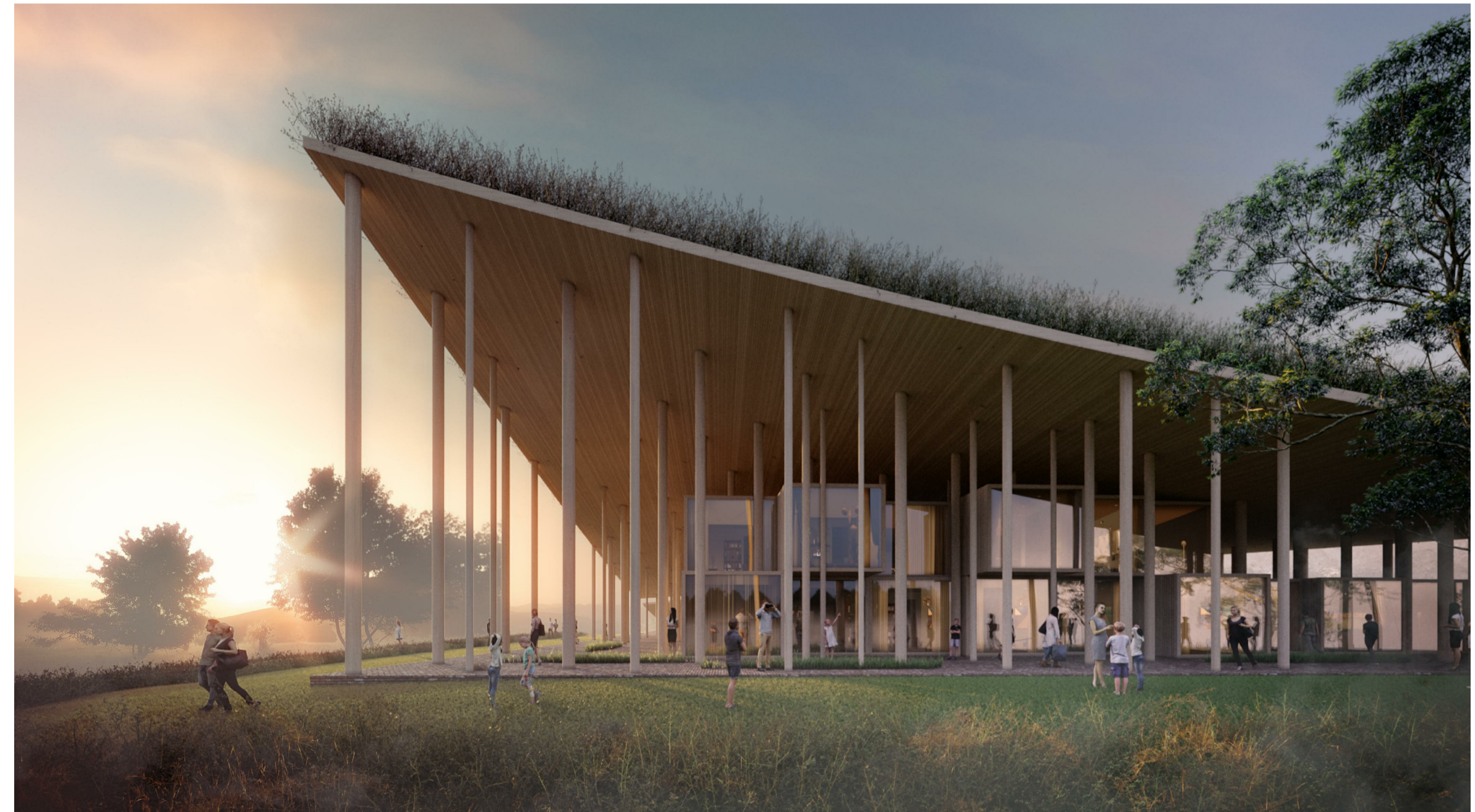
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Job No.
 21-02
 Date
 21.10.2021

EXTERNAL MATERIAL & FINISHES SCHEDULE

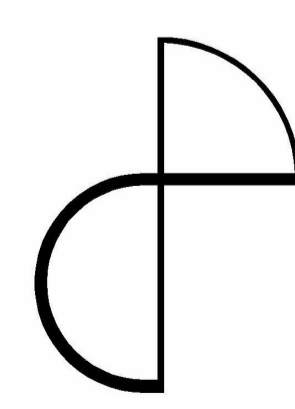
AGRICULTURAL HOTEL DEVELOPMENT

63 MAHERS ROAD, WARRENHEIP



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Job No.
21-02
Date
21.10.2021

PERSPECTIVE IMAGES

AGRICULTURAL HOTEL DEVELOPMENT

63 MAHERS ROAD, WARRENHEIP



29 July 2022

Katy Baker
Coordinator Statutory Planner
Ballarat City Council
PO Box 655
BALLARAT VIC 3353

Via email: planninginfo@ballarat.vic.gov.au

Dear Katy,

Re: Planning Permit No. PLP/2021/626
Address: 63 Mahers Road, Warrenheip
Response to matters raised in the 13 July Delegated Committee Meeting

Glossop Town Planning continues to act on behalf of Forte Group Pty Ltd, the permit applicant in relation to this matter.

As you are aware, this application was considered on 13 July 2022 at the Delegated Committee Meeting. I attended the meeting and presented the town planning merits of the application to the Councillors. A number of issues were raised by the Councillors. It was determined by the Councillors to defer the matter to the next meeting, which I understand will be on 10 August 2022.

The following is a list of the matters for further discussion with the Councillor's provided to me in an email dated 18 July 2022:

1. *Further information regarding how the adjoining agricultural uses will be protected.*
2. *Landscape plan: retention of the cypress tree and details of fences, including proposed heights, materials and locations.*
3. *Concerns relating to the highway, in particular discussion about the intersections and whether or not this application would further contribute to crashes. Any further correspondence with VicRoads, or traffic management plans may assist.*
4. *Reticulated sewerage and water: in particular any external conversations that may have occurred with CHW regarding availability, upgrades, time frames and cost (I know this is predominantly a civil issue, but is a cause for concern with objectors).*
5. *Biosecurity. Options include a land management plan, or as discussed during our call, we can table a biosecurity condition.*

In response to these matters, we provide the following:

How will the adjoining agricultural uses be protected?

Firstly, there are no immediately adjoining properties that are used for agricultural purposes. The only adjoining property is 9367B Western Highway to the west which is within the Mixed Use Zone (MUZ) and forms part of the commercial uses including the



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Amazing Mill Markets (in addition to the Gold Rush Indoor and Outdoor Mini Golf and the Ballarat Colonial Motor Inn further west).

The farm/agricultural land located opposite Greene Road to the east is 24 Greene Road and opposite Mahers Road to the south is part of 26 Mahers Road.

Fencing was raised as a concern and is addressed in the response below.

A Patron Management Plan can be a condition of a permit, that can include steps to advise guests about the presence of operational farms in the area and to behave accordingly. Other mechanisms (such as signage) can reinforce the need to respect local landowners and not interfere with their activities. Staff can also be trained on the need to co-ordinate guest arrivals or deliveries around stock movements on roads and the like.

Landscape plan: retention of the cypress tree and details of fences, including proposed heights, materials and locations.

A Landscape Masterplan prepared by Hansen was submitted with the application and the landscape plan will be required by condition of permit to be endorsed by Council (see Condition 1(b) and Condition 9). Please note that Condition 1(b) should also refer to Condition 18 (i) which relates to Moorabool Shire's Landscape Plan requirements.

There was discussion in relation to the proposed fencing style and height due to a perceived concern about restricting access from the site. We do not consider there to be any security or access concerns. The hotel is located centrally to the site and contains on-site facilities for the guests. There is no obvious reason for guests to try to walk across the site boundaries. However, if there is a type and height of fence that Council would like to be built around the site, this is something that can be required by condition of the permit. It is noted that Condition 9(e) requires: *'details of fences and access gates (both pedestrian and vehicle); including provision of a solid boundary fence, with a minimum height of 1.3m to be provided along the length of Mahers and Greene Road.'*

The following response was provided in our letter in relation to the objector's concerns along the western boundary:

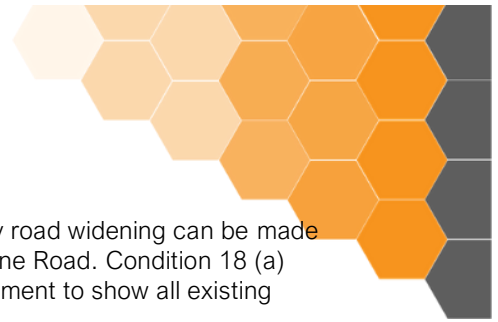
Western Boundary Fence Treatment

The plans do not specify the proposed fencing treatment along the western boundary. The existing traditional post and wire fence would be adequate and most suitable having regard to the surrounding context. There is a substantial setback area along this boundary that is set aside for new tree planting. The buildings and car parking areas are well setback from the western boundary.

In relation to the cypress trees, there are existing rows of trees located within the road reserves (Ballarat Council for Mahers Road and Moorabool Council for Greene Road). More particularly, there are existing rows of cypress 'street trees' along the southern side of Mahers Road and partially along the eastern side of Greene Road opposite the site.

We note Condition 9(b) says *'tree protection zones for all retained trees, including those within the adjoining road reserves, provided in accordance with AS 4970-2009 Protection of trees on development sites'*. Condition 10 relates to 'street trees' and seeks to protect the existing street trees. It requires approval for the removal of any street trees.





In relation to Greene Road upgrade, it was submitted that any road widening can be made towards the site in order to protect the existing trees on Greene Road. Condition 18 (a) relates to the upgrade of Greene Road and includes a requirement to show all existing trees and only the trees shown approved for removal.

It is considered these conditions will adequately protect the existing cypress trees on the opposite sides of the street within the road reserves.

Concerns relating to the highway, in particular discussion about the intersections and whether or not this application would further contribute to crashes. Any further correspondence with VicRoads, or traffic management plans may assist.

Traffix Group have been the project traffic engineers responsible for the traffic engineering matters. We attach a separate response provided by them in relation to this matter and in addition the issue raised on whether the traffic data was collected during a COVID-19 affected time (Traffix Group Memorandum dated 27 July 2022).

Our letter dated 25 March 2022 also set out a response to each of the roads and intersections. In relation to the intersections, it says:

Old Melbourne Road & Mahers Road (VicRoads/Department of Transport and Ballarat City Council)

Improvements to the performance of the intersection will occur as a consequence of the Mahers Road upgrade mentioned above. There is no town planning or traffic engineering reason why this proposal would require the intersection itself to be upgraded.

Old Melbourne Road, Western Freeway and Brewery Tap Road intersection (VicRoads/Department of Transport)

The Department of Transport requested further information from the applicant to provide a complete assessment on any requirements to this intersection. Please refer to the Traffix Group response letter. It confirms no upgrades are required in association with the proposal.

VicRoads/Department of Transport do not object to the proposal, subject to the inclusion of conditions of permit. They are the responsible authority in relation to the Brewery Tap Road intersection.

The Mahers Road upgrade will result in improvements to its intersection with Old Melbourne Road. Council's Traffic and Transport Department did not raise any concerns regarding this intersection.

The attached Memorandum from Traffix Group confirms there have been no recorded accidents at the surrounding intersections in the past 5 years. It also confirms the timing when the traffic data was collected has not been impacted by COVID-19 restrictions.

Reticulated sewerage and water: in particular any external conversations that may have occurred with CHW regarding availability, upgrades, time frames and cost (I know this is predominantly a civil issue, but is a cause for concern with objectors).

In relation to the reticulated sewerage and water connection we note that the Officer recommended permit conditions address this as follows:





Condition 6. Services

a) The development must be connected to reticulated sewerage.

b) The development must be connected to a reticulated potable water supply or have an alternative potable water supply, with appropriate storage capacity, to the satisfaction of the Responsible Authority.

c) The development must be connected to a reticulated electricity supply or have an alternative energy supply to the satisfaction of the Responsible Authority.

Condition 7. Section 173 Agreement - Services connection

Prior to the commencement of use as authorised by this permit, the permit holder must enter into an Agreement with the Responsible Authority, pursuant to Section 173 of the Planning and Environment Act 1987. All costs associated with setting up the Agreement must be borne by the owner. The Agreement must be registered on Title and run with the land, and must provide to the satisfaction of the Responsible Authority:

a) that all buildings are connected to reticulated sewerage and water, services in accordance with the provider's requirements and relevant legislation at the time.

All to the Satisfaction of the Responsible Authority.

It is beyond doubt that the use cannot commence until the site is connected to reticulated sewerage and water.

The site will be required to be connected to reticulated services prior to the commencement of use and this will be bound by a Section 173 Agreement with the Council and will be registered on Title.

Dial before you dig information was obtained in relation to Central Highlands Water (CHW) sewer and water assets.

The project consulting Service Engineer (Simpson Kotzman Consulting Engineers) have advised:

Potable water

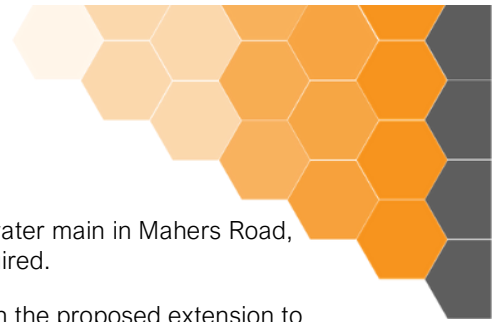
Water pressure at the site will be low as the water authority has not made provision for a standard water supply to the site. This means that water storage will be required for both the domestic water supply and the fire protection systems (fire services and sprinklers). Pumps will be required for each service.

Sewerage

A sewerage mains extension and possible augmentation will be required from the existing Central Highlands authority sewer main on located the southside of the Western Highway some 500 metres or more towards Ballarat near the Ballarat Colonial Inn.

An application with CHW is required to commence the process of arranging a sewer main extension. This process is undertaken by the developer using one of CHW's accredited consultants.





There is potable water provision to the property via existing water main in Mahers Road, however the pressure is low and therefore pumps will be required.

Please refer to the attached Asset Plan provided by CHW with the proposed extension to the sewer main to the site.

The project consulting Service Engineer will continue to provide advice to the developer in relation to what is required for the connection to reticulated water and sewer and any other engineering requirements.

We are advised that the Permit Applicant has considered the cost of providing these services to the site as part its preliminary feasibility.

Biosecurity. Options include a land management plan, or as discussed during our call, we can table a biosecurity condition.

Biosecurity is not a term defined in the planning scheme and is not a matter directly considered in the planning permit process.

Wikipedia provides the following definition:

Biosecurity refers to measures aimed at preventing the introduction and/or spread of harmful organisms (e.g. viruses, bacteria, etc.) to animals and plants in order to minimize the risk of transmission of infectious disease. In agriculture, these measures are aimed at protecting food crops and livestock from pests, invasive species, and other organisms not conducive to the welfare of the human population. The term includes biological threats to people, including those from pandemic diseases and bioterrorism. The definition has sometimes been broadened to embrace other concepts, and it is used for different purposes in different contexts.

There are ways in which the permit can address this concern, either a condition that requires a Land Management Plan be prepared and endorsed under the permit or the permit includes a condition that relates to biosecurity. In principle, the Permit Applicant supports addressing biosecurity in this way, subject to seeing a draft of the proposed condition.

We note that:

- The proposal does not require or directly result in any people entering the nearby agricultural properties.
- The proposed landscaping seeks to use a mix of native/indigenous and exotic species, selected based on aspect and location as well as low fire risk.
- The proposal will remove the existing weeds from the site.
- The operation of the site is not expected to result in any introduction of matter considered in terms of biosecurity.





Conclusion

We trust the above information provides adequate response to the concerns raised by the Councillors in respect to this application.

If you require further information or wish us to engage in a Councillor briefing meeting, please let me know.

If you have any queries, please do not hesitate to contact our office on 9329 2288.

A handwritten signature in blue ink, appearing to read 'John Glossop'.

John Glossop
Director

Glossop Town Planning

Cc. Permit Applicant, Cornetta Partners Architects.
Encl. as stated.



Memorandum



To:	John Glossop (Glossop Town Planning)	From:	Brent Chisholm (Traffix Group)
Our Ref:	G29398M-02A	Date:	Wednesday, 27 July 2022
63 Mahers Road, Warrenheip – Response to Council Concerns			

We refer to the Planning Permit application for a proposed residential hotel at 63 Mahers Road, Warrenheip that was lodged with the City of Ballarat (No. PLP/2021/626).

We understand that Council has queried the potential impact of the COVID-19 pandemic on the traffic count data outlined in our letter titled 'Further Traffic Impact Assessment' dated 24 March 2022 (Ref: G29398L-01B).

Furthermore, Council's email correspondence dated 18th July 2022 also raised a concern regarding the safety of intersections that would be used by motorists visiting the site.

Our assessment of the above is presented following.

Traffic Volumes

Traffix Group commissioned turning movement counts at the Western Freeway/Brewery Tap Road/Old Melbourne Road intersection on Friday 25th February 2022 between 8:00am to 11:00am and 3:00pm to 6:00pm.

To assess the potential of any impact of the COVID-19 pandemic on the recorded traffic volumes, we have sourced historical traffic volumes from SCATS data at the Victoria Street/Water Street/Fussell Street intersection located approximately 3.6km to the west of the site in Ballarat East. This is the nearest traffic signals to the site and serves as the most direct route to the Ballarat City Centre. This exercise has been undertaken for comparative purposes to understand the changes in peak hour traffic volumes over the last few years.

The two-way peak hour traffic volumes recorded at the eastern and western Victoria Street approaches to the signalised intersection are presented at Table 1. This data includes the same day that our counts were undertaken at the Western Freeway/Brewery Tap Road/Old Melbourne Road intersection in February 2022 and also corresponding Fridays in previous years prior to the pandemic.

Table 1: Victoria Street/Water Street/Fussell Street SCATS Summary

Date	AM Peak Hour Volume	PM Peak Hour Volume
Friday February 25 th 2022	568 vehicles	457 vehicles
Friday 31 st January 2020 ¹	332 vehicles	306 vehicles
Friday 22 nd November 2019	235 vehicles	308 vehicles

¹ It is noted that the intersection loop detectors were not functioning on a similar Friday in February 2020, therefore we have sourced SCATS data at an earlier Friday on January 31st 2020.

63 Mahers Road, Warrenheip



This comparative exercise shows that traffic volumes between 2019 and 2022 have actually increased and therefore the February 2022 volumes do not appear to be impacted by the pandemic. Accordingly, the counts that were undertaken at the Western Freeway/Brewery Tap Road/Old Melbourne Road intersection in February 2022 are appropriate and are representative of normal traffic volumes.

We have also reviewed the results of previous turning movement counts that were undertaken at the Western Freeway/Brewery Tap Road/Old Melbourne Road intersection on Thursday 1st June 2017. These counts indicate that traffic volumes were higher in 2022 than 2017 which is consistent with the comparative exercise undertaken above at the nearby traffic signals.

Accordingly, we are satisfied that the recorded traffic count data is appropriate and it does not appear to be impacted by the pandemic.

Road Safety Review

We have obtained State Road Accident Records (Crashstats) for the past 5 years of available data (01/01/2015 to 30/04/2020). The review area is presented at Figure 1.

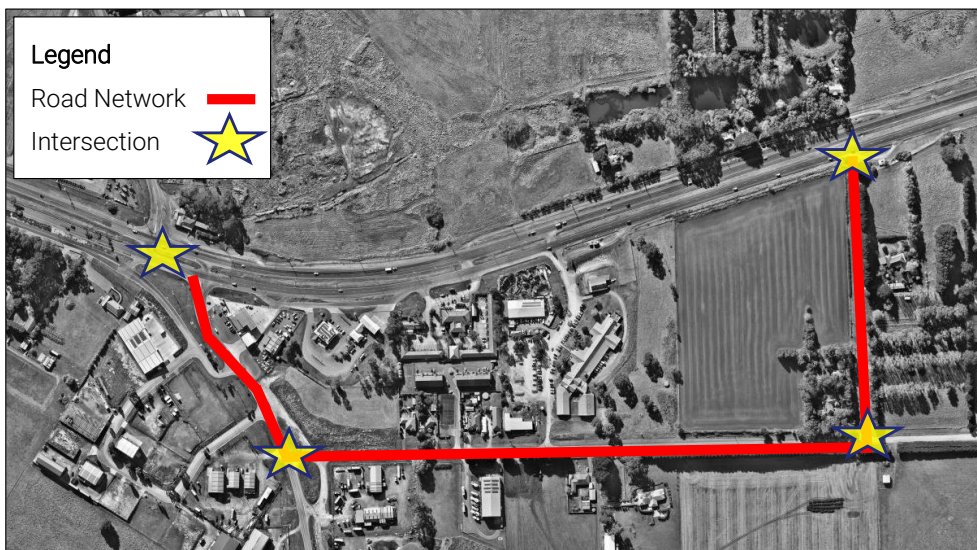


Figure 1: Crashstats Review Area

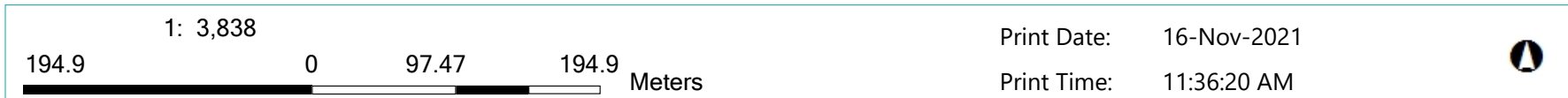
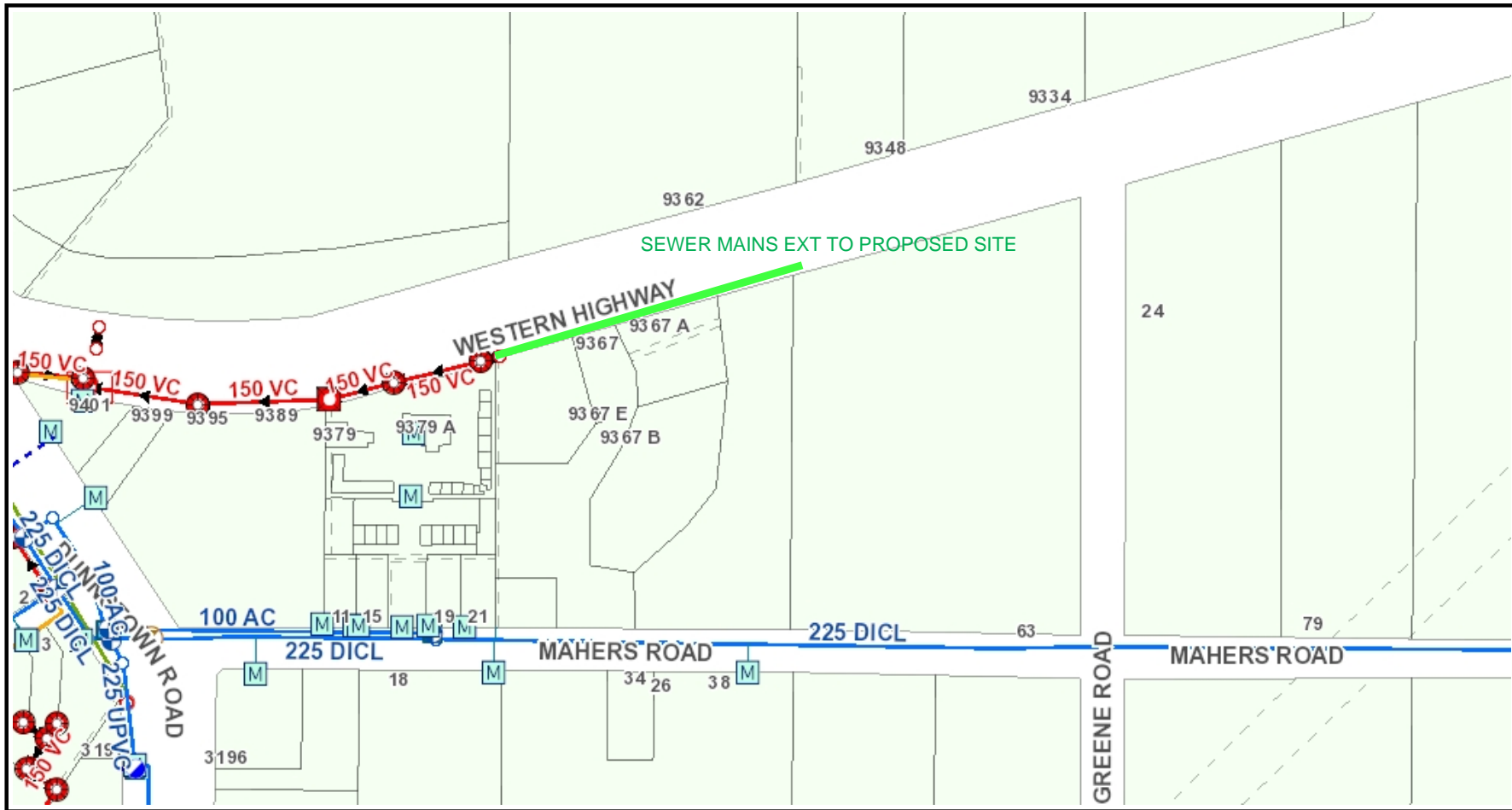
Our review indicates that no crashes were recorded within the last 5 years of available data. Accordingly, there is no reason to suggest that that traffic associated proposed development will result in impacts to the safety of the surrounding road network and intersections.

Accordingly, we are satisfied that there are no road safety concerns associated with the proposed development.





Central Highlands Water



Central Highlands Water has taken care to ensure that the location of pipes and assets shown on this plan are accurate, however, some variations from records do exist and complete accuracy cannot be guaranteed. In all instances it is essential that the position of the pipes concerned be proven on site by field survey and hand excavation. It is an offence to destroy, damage, alter or in any way interfere with any works or property of CHW (Water Act, 1989 Section 288)

Traffix Group

Traffic Engineering Assessment

Proposed Residential Hotel Development
63 Mahers Road, Warrenheip

Prepared for
Forte Group Pty Ltd

September 2021

G29398R-01C

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Traffic Engineering Assessment

63 Mahers Road, Warrenheip

Document Control

Our Reference: G29398R-01C

Issue No.	Type	Date	Prepared By	Approved By
A	Draft	12/08/21	K. Bullock	B. Chisholm
B	Final	27/08/21	K. Bullock	B. Chisholm
C	Updated Final	03/09/21	K. Bullock	B. Chisholm

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Traffic Engineering Assessment

63 Mahers Road, Warrenheip

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Traffic Engineering Assessment

63 Mahers Road, Warrenheip

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Traffic Engineering Assessment

63 Mahers Road, Warrenheip

1. Introduction

Traffix Group has been engaged by Forte Group Pty Ltd to undertake a Traffic Engineering Assessment for the Proposed Residential Hotel Development at 63 Mahers Road, Warrenheip.

This report provides a detailed traffic engineering assessment of the parking and traffic aspects associated with the proposed development.

**Traffic Engineering
Assessment**

63 Mahers Road, Warrenheip

2. Existing Conditions

2.1. Subject Site

The subject site is located on the northwest corner of the Mahers Road / Greene Road intersection in Warrenheip, as shown in the locality plan at Figure 1.

The site has frontages to Western Highway to the north, Mahers Road to the south and Greene Road to the east.

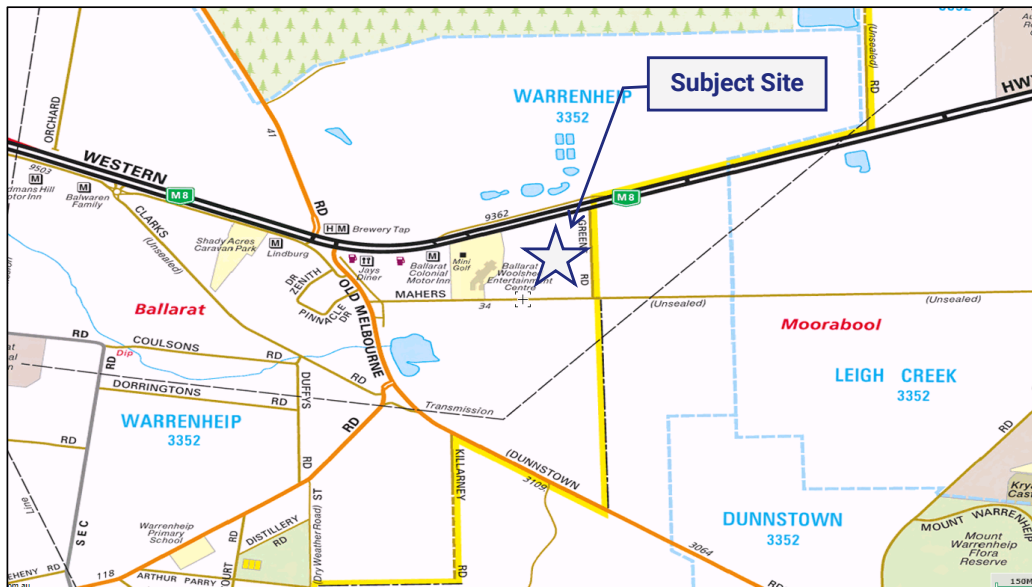


Figure 1: Locality Plan

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The site has a single residential dwelling and associated structures towards the southeast corner of the site, with the remaining area of the site being largely vacant and previously used for agricultural purposes.

The site has an existing crossover to Mahers Road located near the southeast corner of the site.

An aerial photograph of the subject site and its surrounds is presented at Figure 2.

Traffic Engineering Assessment

63 Mahers Road, Warrenheip



Figure 2: Aerial Photograph

Source: Nearmap

The site is zoned 'Farming Zone (FZ)' under the Ballarat Planning Scheme as presented in the land zoning map at Figure 3.

Surrounding land is predominately zoned Mixed Use Zone to the west and Farming Zone in all other directions. A market and motel are located to the west of the site, which are accessed via both Western Highway and Mahers Road.

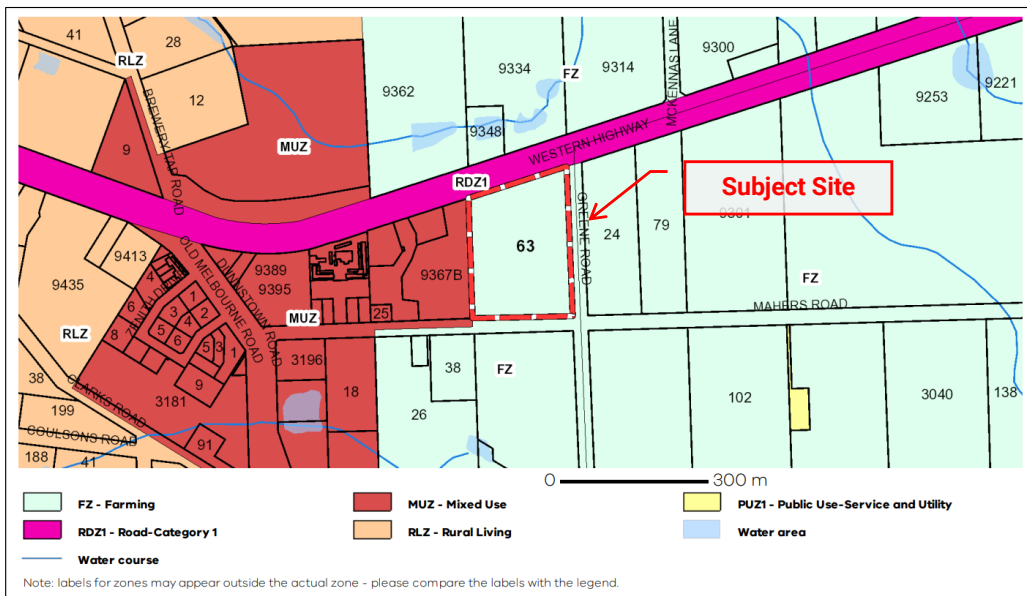


Figure 3: Land Use Zoning Map

Source: VicPlan

Traffic Engineering Assessment

63 Mahers Road, Warrenheip

2.2. Road Network

Mahers Road is a local road under the management of Council that is aligned in an east-west direction between Reidys Road (to the east) and Old Melbourne Road (to the west).

In the vicinity of the site, Mahers Road has an approximate 3.6m wide sealed carriageway plus gravel shoulders on both sides.

The speed limit on Mahers Road is unsigned near the site and therefore the default rural speed limit of 100km/hr applies.

Mahers Road, in the vicinity of the site, is shown at Figure 4 and Figure 5.

Greene Road is a local road under the management of Council that is aligned in a north-south direction between Western Freeway (to the north) and Mahers Road (to the south).

In the vicinity of the site, Greene Road accommodates an approximate 4.2m wide carriageway with unsealed shoulders.

The speed limit on Greene Road is unsigned and therefore the default rural speed limit of 100km/hr applies.

Greene Road, in the vicinity of the site, is shown at Figure 6 and Figure 7.

Western Highway is zoned Road Zone Category 1 and is a highway aligned in a general northwest-southeast direction along the northern boundary of the site.

In the vicinity of the site, Western Highway accommodates two lanes of through traffic in each direction, separated by a central median.

A posted speed limit of 80km/h applies to Western Highway past the site.

The intersection of Western Highway and Greene Road is a T-intersection that accommodates left-turn movements only.

Western Highway, in the vicinity of the site, is shown at Figure 8 and Figure 9.

Traffic Engineering Assessment

63 Mahers Road, Warrenheip



Figure 4: Mahers Road - view west



Figure 5: Mahers Road - view east



Figure 6: Greene Road - view north



Figure 7: Greene Road - view south



Figure 8: Western Highway (westbound carriageway) - view west



Figure 9: Western Highway (westbound carriageway) - view east

Traffic Engineering Assessment

63 Mahers Road, Warrenheip

3. Proposal

The proposal is for a residential hotel development at 63 Mahers Road, Warrenheip.

The development will include the following:

- a residential hotel with 119 rooms,
- an ancillary function area accommodating a maximum of 140 patrons,
- an ancillary restaurant accommodating a maximum of 80 patrons,
- an ancillary bar, and
- an ancillary agricultural area and gin distillery.

Vehicle access for the proposed residential hotel is to be provided as follows:

- Separate entry and exit only connections with Greene Road on the eastern boundary of the site which will provide access to the hotel drop-off / pick-up area,
- A two-way connection with Greene Road which will provide access to the main carpark and hotel loading area,
- A two-way connection with Mahers Road which provides access to the main carpark and bus parking bays, and
- A two-way connection with Mahers Road which provides access to the 'Orchard Shed'.

An on-site loading bay is to be provided for deliveries and waste collection. This loading bay is to be accessed via the main carpark with trucks entering and exiting via the southern access connection with Greene Road.

A total of 174 on-site car parking spaces are to be provided as follows:

- 171 car spaces in the main car park including two (2) disabled spaces, and
- Three (3) drop-off/pick-up car spaces near the hotel entrance.

Additionally, two (2) bus parking bays are to be provided towards the eastern side of the carpark, capable of accommodating up to 14.5m long buses/coaches. Furthermore, a bus drop-off/pick-up bay is to be provided near the main hotel entrance.

Bicycle parking is to be provided on-site for staff in a secure bike store that is located within the hotel building and for visitors near the lobby.

A copy of the proposed development plans, prepared by Cornetta Partners Architects, is attached at Appendix A.

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4. Traffic Engineering Assessment

4.1. Car Parking Assessment

4.1.1. Statutory Car Parking Requirements

The proposed development falls under the land-use category of 'residential hotel' and 'agriculture' under Clause 73.03 of the Planning Scheme.

The function centre (140 patrons), restaurant (80 patrons) and bar are ancillary to the operation of the residential hotel and therefore will not generate a separate requirement for car parking. Similarly, the gin distillery is ancillary to the agricultural use and therefore does not generate a separate requirement for car parking.

The Planning Scheme sets out the parking requirements for new developments under Clause 52.06.

The purpose of Clause 52.06 is:

- *To ensure that car parking is provided in accordance with the Municipal Planning Strategy and the Planning Policy Framework.*
- *To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality.*
- *To support sustainable transport alternatives to the motor car.*
- *To promote the efficient use of car parking spaces through the consolidation of car parking facilities.*
- *To ensure that car parking does not adversely affect the amenity of the locality.*
- *To ensure that the design and location of car parking is of a high standard, creates a safe environment for users and enables easy and efficient use.*

There is no specified car parking rate under Table 1 of Clause 52.06-5 of the Planning Scheme for a 'residential hotel' or 'agriculture' use. Accordingly, car parking spaces must be provided to the satisfaction of the Responsible Authority as stated at Clause 52.06-6.

The statutory car parking requirement for the proposal based on the car parking rates set out in Clause 52.06 is provided at Table 1.

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Table 1: Statutory Car Parking Requirement (Clause 52.06)

Use	Measure	Parking Rate (Column A)	Parking Requirement
Residential Hotel	119 rooms	To the satisfaction of the responsible authority	N/A
Agriculture	N/A	To the satisfaction of the responsible authority	N/A

Based on the above, car parking must be provided to the satisfaction of the responsible authority.

An assessment of the anticipated car parking demand to be generated by the proposed development is discussed in the following section.

4.1.2. Car Parking Demand Assessment

Residential Hotel

Given the site's location, we expect that the majority of guests and staff will travel to/from the site via a private motor vehicle. Furthermore, a proportion of guests may travel via taxi or rideshare, therefore will not generate any car parking demand.

We also understand that the hotel operator anticipates that some guests will arrive as part of tour groups travelling via coaches, smaller buses, minivans, etc. Accordingly, these guests will not generate a demand for car parking. There is also the likelihood that some families travelling in a single car will stay at the hotel and book more than one room.

We consider it appropriate to adopt a peak car parking rate of 1 car space per room, inclusive of staff and guest car parking demands.

On this basis, the residential hotel component of the development is expected to generate a demand for up to 119 car spaces.

Function Space

As outlined previously, the function space is considered ancillary to the operation of the residential hotel under the land-use term at Clause 73.03 of the Planning Scheme and therefore will not generate a separate requirement for patrons.

There is a high likelihood that patrons of the function space would also stay at the proposed hotel and therefore not generate a separate demand for car parking.

However, it is expected that additional staff associated with the function space will require on-site car parking during events. We expect that the provision of 20 car spaces for function space staff will sufficiently accommodate any staff demands.

Restaurant

The restaurant is considered ancillary to the operation of the residential hotel.

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Nevertheless, we have conservatively assessed the car parking demand likely to be generated at a rate of 0.4 spaces per patron, which is the same rate as a standalone restaurant. Based on this conservative assumption the restaurant would require 32 car parking spaces, inclusive of staff and customer demands.

Bar

As mentioned previously, the bar is considered to be an ancillary use of the residential hotel. Therefore, we do not expect there to be any additional car parking demands.

Agriculture and Gin Distillery

The agricultural component of the site is intended to be used solely for the purposes of ingredients for the gin distillery.

Any car parking demands for these components will be accommodated informally within the area adjacent to the orchard shed.

Summary

Based on the above assessment, a conservative peak total car parking demand for 171 spaces is predicted. Therefore, the proposed car parking provision of 174 spaces exceeds the anticipated peak demand.

4.2. Bicycle Parking

Clause 52.34 of the Planning Scheme specifies bicycle parking requirements for new developments and changes in use.

The land-use category 'Agriculture' is not listed in Table 1 of Clause 52.34, therefore there is no statutory requirement to provide bicycle parking.

It is noted that the 'Residential hotel' land-use category falls under the broader land-use category of 'residential building'. The statutory bicycle parking requirement for a residential building is only applicable to developments that are four or more storeys. Accordingly, there is no statutory requirement to provide bicycle parking for the proposed development.

Nevertheless, a total of two (2) on-site bicycle parking spaces are to be provided for staff in a secure store area at ground level.

A single double-sided bicycle parking hoop (i.e. 2 spaces) is also proposed near the hotel lobby for visitors.

Based on the above, we are satisfied that the proposed bicycle parking provision is appropriate.

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4.3. Car Parking Layout and Access Arrangements

Traffix Group has provided design advice to the project architect to achieve a satisfactory layout for parking and access arrangements. The proposed parking layout has been assessed under the following guidelines:

- Clause 52.06-9 of the Planning Scheme (Design standards for car parking), and
- The relevant Australian Standards.

A total of 174 on-site car parking spaces are to be provided including 171 car spaces within the main carpark and three (3) spaces within the drop-off/pick-up area.

Access to the hotel drop-off / pick-up area is to be provided via separate entry and exit crossovers with Greene Road.

The hotel carpark is to be accessed via dual-width two-way connections with Greene Road and Mahers Road.

A separate crossover is to be provided with Mahers Road which provides access to the 'Orchard Shed'.

Key elements of the design include:

4.3.1. Car Spaces

- Car spaces at 90 degrees are to be provided at a width of 2.6m, length of 4.9m and are provided with an adjacent access aisle of at least 6.4m which accords with the Planning Scheme dimensions under Clause 52.06-9 (Design Standard 2).
- Two (2) disabled car spaces are to be provided at a width of 2.4m, length of 5.4m and with an adjacent shared area of the same dimensions in accordance with the Australian Standard for disabled parking (AS/NZS 2890.6:2009).
- Parallel car spaces along the drop-off/pick-up area are to be provided at a width of 3.5m, length of 6.7m and with an adjacent access aisle of at least 3.6m which accords with the Planning Scheme dimensions under Clause 52.06-9 (Design Standard 2).
- Access to and from each of the critical to access car spaces has been checked for the 85th percentile design vehicle using the computer simulation program AutoTurn and has been found to be satisfactory.

4.3.2. Access Arrangements

- The Porte Cochere is proposed to operate in a one-way direction with entry via the northern Greene Road crossover and exit via the southern Greene Road crossover.
- Access to/from the Porte Cochere has been checked for swept paths using a 14.5m long bus and found to be acceptable.
- Vehicles can enter and exit the site in a forward direction in accordance with Clause 52.06-9 (Design Standard 1).

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- Both crossovers to the hotel carpark are to be provided at a width of 6.4m which is sufficient to accommodate simultaneous two-way vehicle movements. Crossover splays should be designed based on swept paths at the detailed design stage.

4.3.3. Bus Parking

- Two (2) bus parking bays are to be provided at the western end of the carpark which are measured to be 15.5m long and 3.5m wide.
- Buses are required to enter and exit the bus parking bays via the Mahers Road car park crossover. This arrangement requires buses to reverse into the bays from the car park.
- Swept path diagrams demonstrating bus movements for a 14.5m long bus throughout the site and to/from parking bays are attached at Appendix B.

Based on the above, we are satisfied that the layout of the proposed parking spaces is satisfactory and the access arrangements for the site will provide for safe and efficient movements to and from the surrounding road network.

4.4. Traffic Impact

4.4.1. Traffic Generation

Hotel staff and guest arrivals/departures are expected to be spread throughout the morning and afternoon/evening periods and will not necessarily coincide with the commuter peak hours. Similarly, staff vehicle movements associated with the restaurant use will likely not coincide with the commuter peak hours.

Since the restaurant and function centre will not operate during the AM peak period, traffic generated during this period is expected to be low and comprise of some hotel guest departures and staff movements only.

During the PM peak period, there may be some traffic associated with staff and patron arrivals for the function centre and restaurant. In addition, there is expected to be some hotel guest and staff arrivals/departures during this period.

For the purposes of our assessment of the critical PM peak hour, we will conservatively assume 50% turnover of the peak predicted car parking demand (171 car spaces) will generate either a vehicle arrival or departure movement during the PM peak hour. This equates to approximately 86 vehicle movements per hour, or approximately only 1.4 vehicle movements per minute on average.

This level of peak hour traffic is relatively low in traffic engineering terms and we are satisfied can be adequately accommodated by the proposed access arrangements and the surrounding road network and intersections without any unacceptable impacts.

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4.4.2. Traffic Distribution

The majority of vehicles that are travelling to the site from Melbourne via the Western Highway are anticipated to use Greene Road to access the site.

It is expected that all vehicles that are travelling to the site from Ballarat will turn right onto Old Melbourne Road, then travel along Mahers Road to access the site.

Departing vehicles are expected to be split evenly between Greene Road and Mahers Road to access Western Highway. Whilst the Old Melbourne Road / Western Highway intersection restricts right-turn movements for vehicles travelling along Old Melbourne Road, a median break is available at Brewery Tap Road only 50m further to the west of this intersection which allows vehicles to U-turn to travel eastbound towards Melbourne.

4.4.3. Hotel Drop-off and Pick-up Car Parking Assessment

For hotel guests there are specific demands associated with drop-off and pick-up and activities which typically occur at the following times:

- Check-out – 8am-11am
- Check-in – 2pm-5pm

A total of three (3) parallel car spaces are proposed along the Porte Cochere for the hotel.

These spaces are ideal for drop-off/pick-up purposes including for taxis, Uber (or similar) or other guest car arrivals.

An assessment of the expected demand for drop-off/pick-up car parking is provided following.

Traffic Group previously undertook a case study of the Olsen Hotel (Chapel Street, South Yarra). This residential hotel includes a total of 231 rooms (compared to 119 rooms for the proposed hotel). As part of this case study, a survey was undertaken on Friday 27th October 2017 between 9am-11am and 2pm-4pm. The survey recorded all cars either stopping on-street within Daly Street adjacent to the hotel or using the Porte Cochere accessed via Daly Street. The survey identified the following key results:

- 9am-11am – 34 separate cars undertaking pick-up/drop-off activity (68 vehicle movements) with a maximum of 4 cars simultaneous undertaking pick-up/drop-off activity at any time.
- 2pm-4pm – 38 separate cars undertaking pick-up/drop-off activity (76 vehicle movements) with a maximum of 4 cars simultaneous undertaking pick-up/drop-off activity at any time.

The survey suggests a peak demand for 4 vehicles at any time in association with drop-off/pick-up activity. Based on the number of rooms within The Olsen Hotel, this equates to a drop-off/pick-up car parking generation rate of 0.017 spaces per hotel room.

Applying this rate to the proposed development equates to a peak demand for 2-3 drop-off/pick-up vehicles at any one time. This peak demand can be adequately accommodated within the three (3) spaces with the drop-off/pick-up lane.

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4.5. External Road Network Upgrades

As discussed previously, the proposed use is expected to generate traffic to/from the north along Greene Road and also to/from the west along Mahers Road. To accommodate this additional level of traffic, it is anticipated that the following upgrades to the external road network will be required:

- The Western Highway / Greene Road intersection is to be upgraded with a left-turn deceleration lane on Western Highway. Furthermore, the Greene Road carriageway is to be widened to accommodate a 6.2m wide seal for a distance of approximately at least 50 metres from Western Highway.
- Along the remaining length of Greene Road, widened gravel passing areas are to be provided at intervals of approximately 50-100 metres, to the satisfaction of Council.
- The existing sealed carriageway of Mahers Road is proposed to be retained. In a similar arrangement to what is proposed for Greene Road, gravel passing areas are proposed at intervals of approximately 50-100 metres between Greene Road and Old Melbourne Road, to the satisfaction of Council.
- Regarding vehicle access crossovers with Greene Road and Mahers Road, these should be designed at the detailed design stage of the project to accommodate relevant design vehicle swept paths.

4.6. Waste Collection and Loading

Clause 65.01 of the Planning Scheme states that the responsible authority must consider a number of matters as appropriate including:

The adequacy of loading and unloading facilities and any associated amenity, traffic flow and road safety impacts.

An on-site loading bay and adjacent turning area is identified on the southern side of the building with access via the hotel carpark.

A bin store is located adjacent to the loading area. Waste collection is to be undertaken via the loading bay.

We are satisfied that suitable on-site loading and waste collection arrangements are to be provided to accommodate the swept paths of up to an 8.8m long medium rigid vehicle (MRV). Swept path diagrams demonstrating satisfactory access to/from the loading bay for an MRV truck is attached at Appendix B.

It would also be suitable for some smaller delivery vehicles such as vans and utilities to use the hotel drop-off/pick-up area deliveries where appropriate.

Based on the above, we are satisfied that the proposed loading and waste collection arrangements are satisfactory.

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5. Conclusions

Having undertaken a detailed traffic engineering assessment for the proposed residential hotel development at 63 Mahers Road, Warrenheip, we are of the opinion that:

- a) there is no specified statutory car parking rate under Clause 52.06 of the Planning Scheme for a 'residential hotel' land-use and therefore car parking must be provided to the satisfaction of the responsible authority for that component,
- b) the proposed on-site car parking provision is satisfactory to meet the anticipated car parking demands for the hotel operation (inclusive of patrons and staff),
- c) the level of on-site car parking is also suitable to accommodate the car parking demands of the restaurant, agricultural, and ancillary gin distillery and function centre uses,
- d) the proposed parking layout and access arrangements accord with the requirements of the Planning Scheme, relevant Australian Standards and current practice,
- e) suitable bicycle parking provisions are to be provided,
- f) the proposed on-site loading bay and access arrangements are satisfactory to accommodate the largest anticipated trucks,
- g) suitable waste collection arrangements can be accommodated via the on-site loading bay,
- h) a suitable level of external road upgrades works are to be provided as outlined at Section 4.5 of this report,
- i) the level of traffic generated as a result of this proposal will be adequately accommodated by the proposed site access connections and the surrounding road network and intersections without any unacceptable impacts, and
- j) there are no traffic engineering reasons why the proposed residential hotel development at 63 Mahers Road, Warrenheip should not be approved.



Appendix A

Proposed Development Plan